

CITY OF NEWPORT
STREETS/RIGHTS-OF-WAY
NEWPORT, WASHINGTON

CONSTRUCTION
STANDARDS

July 2022

Prepared For:

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City Administrator
For the City of Newport

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CONSTRUCTION REQUIREMENTS

The standards of this section primarily apply to installations in paved areas within public rights-of-way. The City encourages trenching for franchise utilities to occur outside of paved areas, in which case the following pavement restoration standards would not apply. For work in paved areas, the following standards shall apply.

A. General

Materials and workmanship shall be in conformance with the adopted standards of the City of Newport, including the most recent version of the WSDOT/APWA Standard Specifications for Road, Bridges, and Municipal Construction. Construction shall be in conformance with the details and conditions outlined in the Construction Permit and with the following. The City Administrator and/or Public Works Director reserves the right to use discretion in this matter when deviations are in the best public interest.

1. No pavement cuts of any kind are allowed in pavement less than 10-years old.
2. Windows may be allowed in pavement more than 10-years old. A Class B asphalt overlay is required for all windows in the traveled way.
3. Open cuts may be allowed, on a case-by-case basis depending on traffic issues, for pavement that is more than 10-years old. A Class B asphalt overlay is required for all open cuts in the traveled way.
4. The “traveled way” is defined as the area of pavement from curb to curb. When there are no curbs, then the traveled way is defined as the area of pavement from fog-line to fog-line. When the paved roadway is without permanent markings, the travel way is defined as a 12' width of pavement on each side of a determined centerline of the street. Paved shoulders outside of the fog line are not considered to be in the traveled way.
5. Street crossings for public utilities shall be pushed or bored whenever possible. When existing utilities are located in the push or bore vicinity in pavement more than 10-years old, a window may be excavated to reveal the exact location of the existing utility. The window shall be the smallest size possible that insures that the push or bore will not damage the existing utility. Existing windows shall be used whenever possible.
6. Excavation or tunneling under sidewalks is not allowed. Sidewalks shall be sawcut, removed, and replaced at a location approved by the City Public Works Director or designee. The sidewalk replacement shall conform to city standards.
7. Maintain two-lanes of traffic whenever possible. A minimum of one-lane of alternating two-way traffic shall be maintained at all times with appropriate signs and flaggers. NO street closures will be allowed without approved traffic control plan.
8. A pedestrian detour route shall be clearly delineated whenever sidewalks are obstructed.

B. Backfill

1. A backfill material for transverse trenches shall be imported 3/4" minus crushed rock or Controlled Density Fill (CDF) conforming to the Standard Specifications. Native backfill will not be allowed. Sand, gravel screenings, or pea gravel may be used within the pipe zone for bedding. Crushed rock or CDF shall then be placed and compacted in the remainder of the excavation.
2. Trench backfill for longitudinal trenches shall be imported unless the City Public Works Director or designee determines that the native material is suitable. The top 8" of backfill shall be crushed surfacing top course in any case.
3. All trench backfill shall be compacted to 95% maximum density as described in the APWA Standard Specifications.
4. The City Public Works Director or designee may require compaction testing of trenches and/or paving. Testing shall be performed by an independent certified testing laboratory. The cost of testing is the responsibility of the franchise utility or the contractor. Compaction of all lifts of asphalt shall be per the APWA Standard Specifications. The number of tests required per square foot of trenching shall be as follows unless directed otherwise by the City:
 - a. One (1) test for less than 50-square feet of trenching area.
 - b. Two (2) tests for 50 to 100-square feet of trenching area.
 - c. Three (3) tests for 101 to 300- square feet of trenching area.
 - d. One (1) test for every 200-square feet over 300-square feet of trenching area or every 100- lineal feet of trench, if applicable.
5. Trenches or other excavations 4' or more in depth that do not meet the open pit requirements of the latest edition of the WSDOT Standard Specifications shall be shored. The contractor shall be solely responsible for worker safety and damages related to shoring or failure to shore.

C. Restoration

1. Trench restoration shall be accomplished with a patch or an overlay as required by the City.
2. All trench and pavement cuts shall be made by sawcuts or by grinding. The sawcuts or grinding shall be a minimum of 1' outside the trench width. If a patch is allowed, the trench limits shall be a sawcut prior to final patching.
3. The replacement pavement section shall include a minimum of 8", compacted depth, crushed rock top course and 4" Class B asphalt.
4. All patching and paving shall be asphalt Class B, placed and compacted in two (2) lifts. Emulsified asphalt tack shall be placed on all cut pavement edges. All

pavement joints shall be sealed with rubberized asphalt sealer. All materials shall conform to the Standard Specifications.

5. Trenches and excavations within paved areas must be patched at the close of each workday. Cold mix asphalt may be placed temporarily. Cold patches shall be maintained constantly until permanent patching is complete. Permanent patching with hot mix asphalt sealer, including rubberized asphalt sealer, must be completed within a time frame determined by the City, usually two (2) weeks, unless traffic conditions warrant paving sooner as determined by the City. Overlays, when required, shall be completed in a timely manner or within a time frame determined by the City as stated on the Construction Permit. Since it is more cost effective to pave multiple locations at one time, the City may allow an extension of time to complete overlays. The City may allow the time frame to be adjusted if paving delays are due to inclement weather or other adverse conditions.
6. Disturbed unpaved shoulder areas shall be compacted to 95% maximum density and restored to original or better condition. At a minimum, 3/4" minus crushed rock shall be placed at a minimum compacted depth of 2".

D. Overlays

A Class B asphalt overlay shall be required for all open cuts and new "windows" in the traveled way. The existing pavement shall be ground to a depth of 1.5". Joint edges shall be sealed with rubberized asphalt sealer. The overlay shall extend transversely across the entire lane width and longitudinally for a distance determined by the City on a case-by-case basis. For windows, the minimum longitudinal distance shall be 4' on each side of the excavation.

E. Conduit & Ductbanks for Telecommunications

1. Background

The City of Newport has identified a need to develop standards that will facilitate the cost-effective installation of underground infrastructure while preserving the integrity and safety of public rights-of-way.

2. Purpose

The purpose of these conduit and ductbank standards will be to:

- a. Facilitate the installation of state-of-the-art telecommunications facilities for the City of Newport and Pend Oreille County.
- b. Minimize the duration and frequency of disruption to the public caused by construction within the rights-of-way.
- c. Allow and require, as necessary, the installation of empty conduit for future use by various providers.

- d. Establish a moratorium on right-of-way use permits that require trenching following street construction or resurfacing.
- e. Provide uniform trenching and conduit installation guidelines for utility companies, telecommunications providers, and developers.
- f. Outline trenching and pavement restoration standards.

3. Deviations

Deviations from these standards may be authorized by the City as conditions warrant. Emergency situations will be handled on a case-by-case basis.

4. Planning and Coordination

It is incumbent on the individual utility companies and telecommunications providers to initiate contact with the City and to be aware of any projects in the City's Six Year Transportation Improvement Program, Capital Improvement Program, or on-going developer activities that may coincide with their planned installation(s). The City will attempt to facilitate these communications insofar as possible, but the responsibility rests with the providers and failure to be aware of, and coordinate with, other projects will not become a valid reason for providers to request a deviation from the moratorium on pavement cutting.

5. Joint Trenching

In order to reduce damage to city streets, minimize disruption to the public, and encourage cost-effective installation of utilities and conduits, the City may require joint trenching. This concept may be initiated in a variety of ways such as:

- a. A utility company applies for a permit to install facilities in the right-of-way.
 - 1) the City may notify other companies that they need to occupy the same trench, or
 - 2) the City may furnish pipe and/or conduit to be included at the same time,
 - 3) the City may require that the applicant install additional empty conduit for future use.
- b. The City initiates a Capital Improvement Project.
 - 1) the City may elect to install its own pipes and/or conduit that can be leased to providers for future use, or
 - 2) the City contacts other companies to give them an opportunity to install their facilities in the right-of-way at the same time in order to avoid a moratorium on pavement cutting.
- c. For new developments, subdivisions, street construction, reconstruction, or commercial sites, the City may require developers to install additional empty ductbanks, conduits, pull-boxes, and related hardware to facilitate the future installation of the state-of-the-art telecommunications. Since the industry is changing rapidly, the City Staff will consult with potential providers in order to determine the details of the required installation.

In all cases, the City will make the determination as to when joint trenching is in the overall public interest.

Some of these situations may provide an opportunity for the installer to recover a portion of their cost through a delayed benefit agreement or similar cost-recovery process. Delayed benefit agreements must be approved in advance by the City Council.

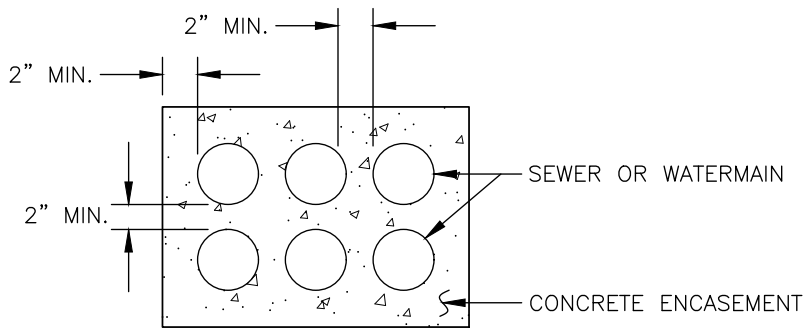
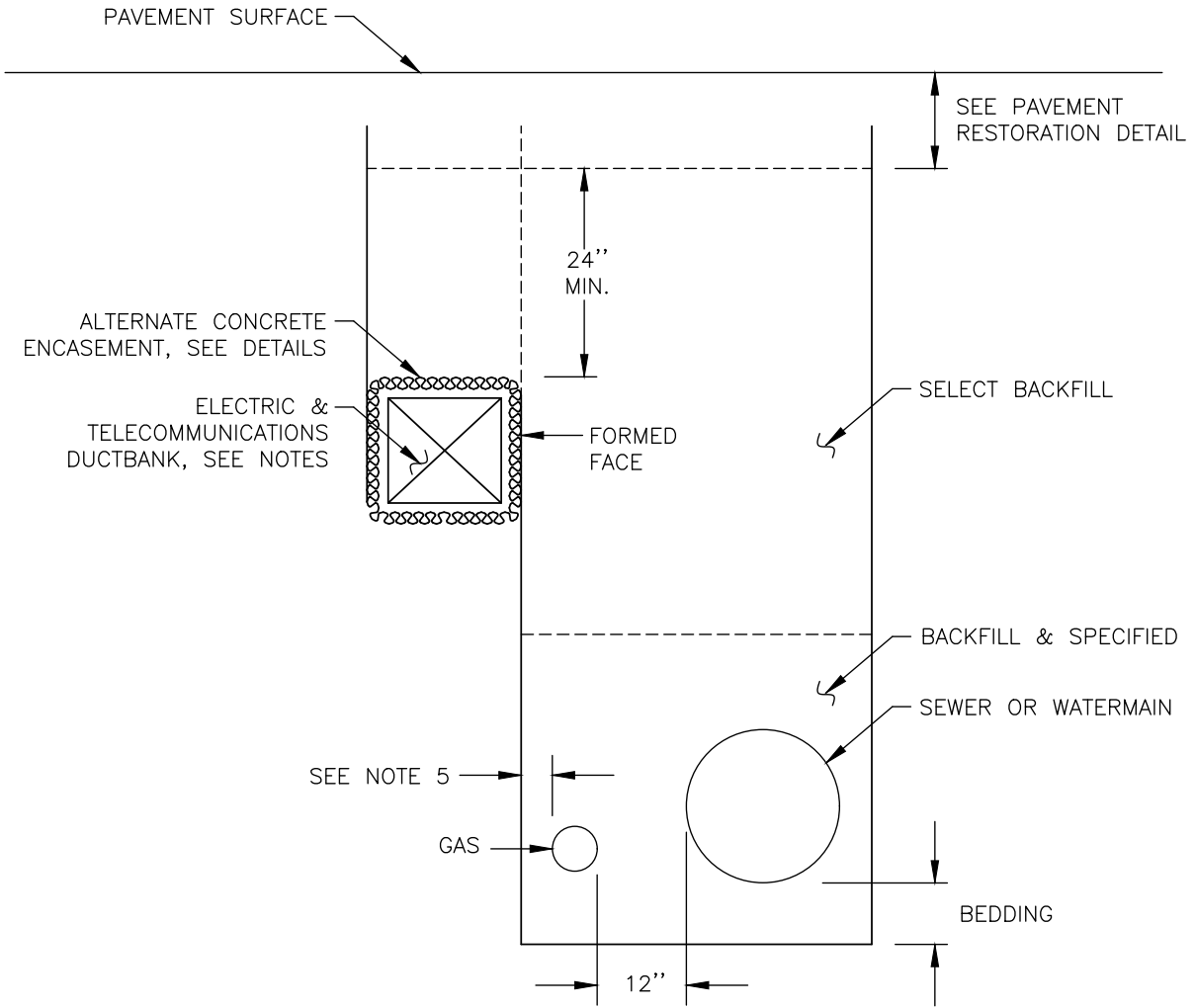
6. Moratorium on Pavement Cutting

No pavement cuts of any kind are allowed in pavement less than 10-years old. Exceptions to this may be authorized by the City on a case-by-case basis and would take into consideration emergency situations and/or cases where the overall public interest would be served by allowing a deviation from this standard.

F. Ductbanks for Buried Cable

1. Separation between communication cables and power, or other electrical conductors, shall be a minimum of 12".
2. The minimum depth of cover in main trenches shall be 24" and 12" in service trenches unless utility regulations dictate otherwise.
3. Trench depths and alignment may require variations to accommodate pull boxes, vaults, manholes, etc. Conduit sweeps generally require a 36" radius.
4. Concrete encasement for ductwork shall meet the City specifications for controlled density fill. Iron oxide red dye may be required for tinting.
5. Generally, on trenches to be shared with sewer or water lines, the alignment and grade of the pipelines will govern.
6. Unique field conditions, accepted industry standards, or utility regulations required by law may require exceptions to any of the aforementioned mentioned installation standards.

STANDARD DETAILS

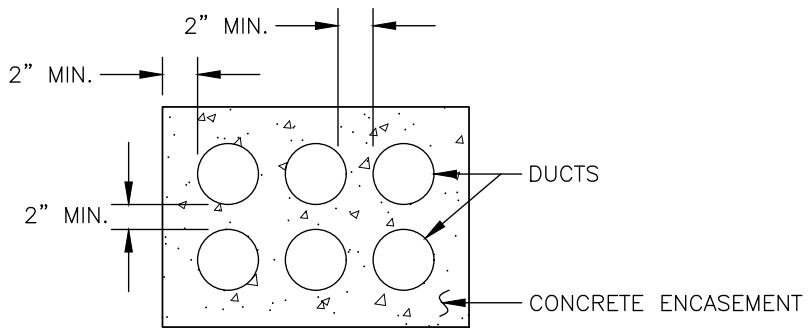
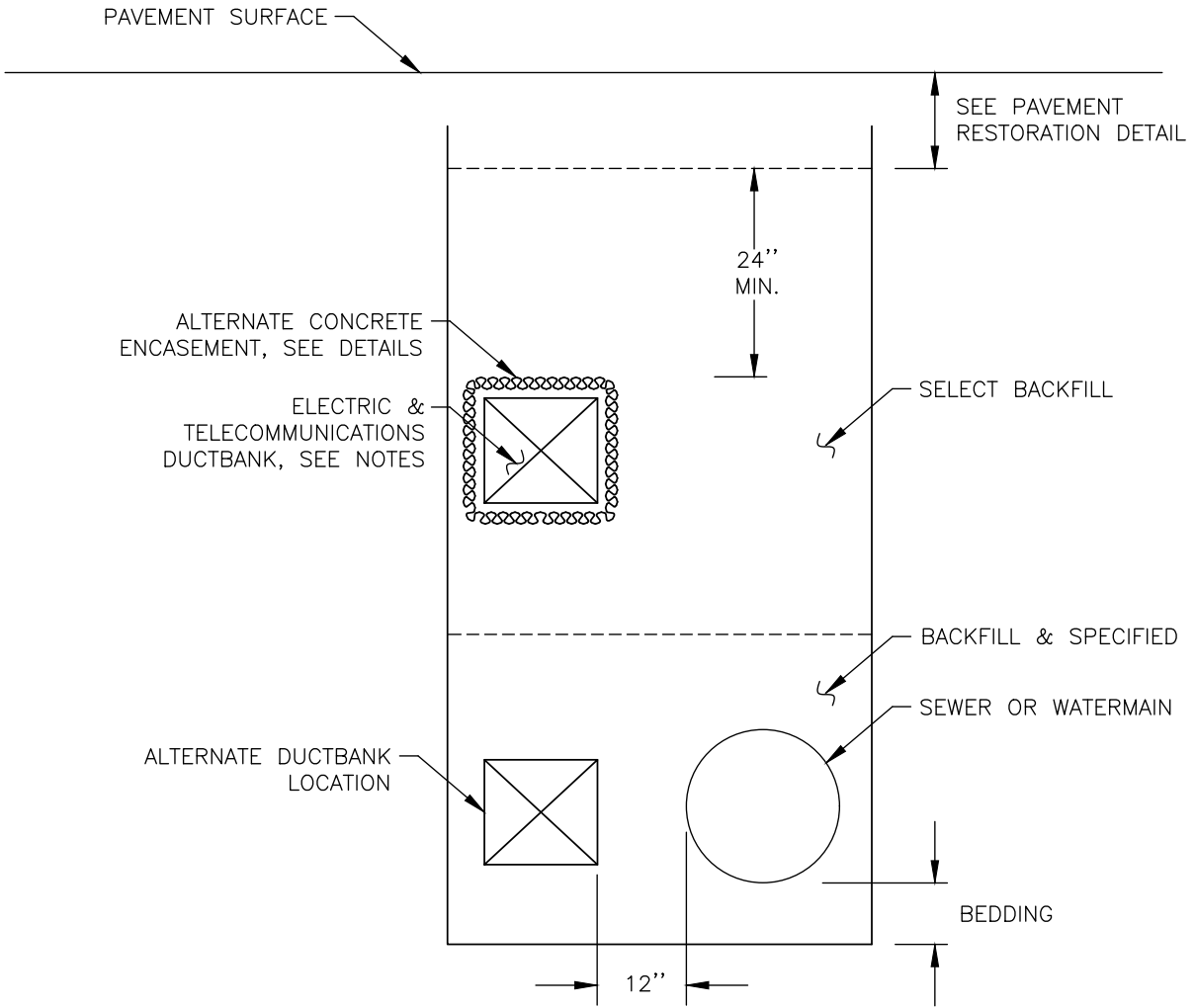


DUCT ENCASEMENT DETAIL

NOTES:

- 1) MAINTAIN 2" CONCRETE WALL THICKNESS AROUND DUCTBANK.
- 2) CONDUIT SEPARATION 2" MINIMUM WITHIN DUCTBANK.
- 3) CDF MIX AND TINT AS REQUIRED BY CITY.
- 4) SEWER OR WATER PIPE WILL DICTATE ALIGNMENT.
- 5) MINIMUM PER UTILITY COMPANY REQUIREMENTS.

| | | |
|---|--------------------------|---------------|
| JOINT TRENCHING OPTION 1 | SCALE: NTS | DATE: 7-01-22 |
| | | |
| CITY OF NEWPORT STANDARD STREET DETAIL | SHEET NO. TR-1 | |

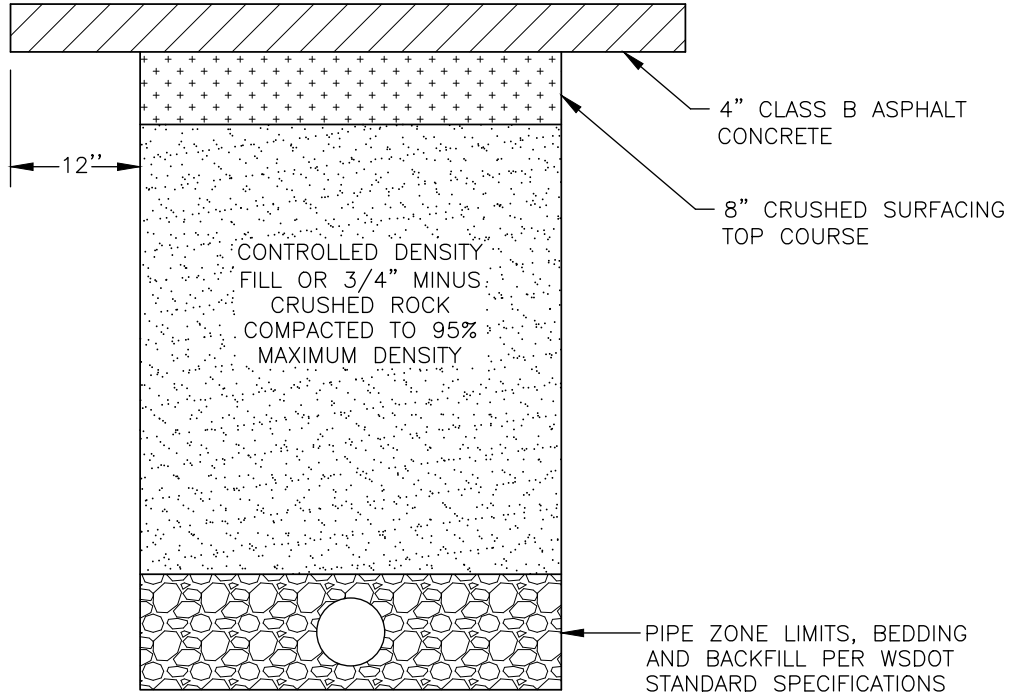


DUCT ENCASEMENT DETAIL

NOTES:

- 1) DUCTBANKS MAY BE INSTALLED IN BATH LOCATIONS IF REQUIRED FOR SEPARATION.
- 2) SEWER OR WATER PIPE WILL DICTATE ALIGNMENT.

| | | |
|-----------------------------|--|--------------------------|
| JOINT TRENCHING OPTION 2 | SCALE: NTS | DATE: 7-01-22 |
| | CITY OF NEWPORT STANDARD STREET DETAIL | |
| | | SHEET NO. TR-2 |



NOTES:

- 1) SAWCUT ALL ASPHALT. CUT ASPHALT BACK 12" FROM EDGE OF TRENCH.
- 2) PLACE TACK COAT ON THE FACE OF ALL CUT PAVEMENT EDGES.
- 3) SEAL ALL JOINTS WITH RUBBERIZED ASPHALT SEALER.

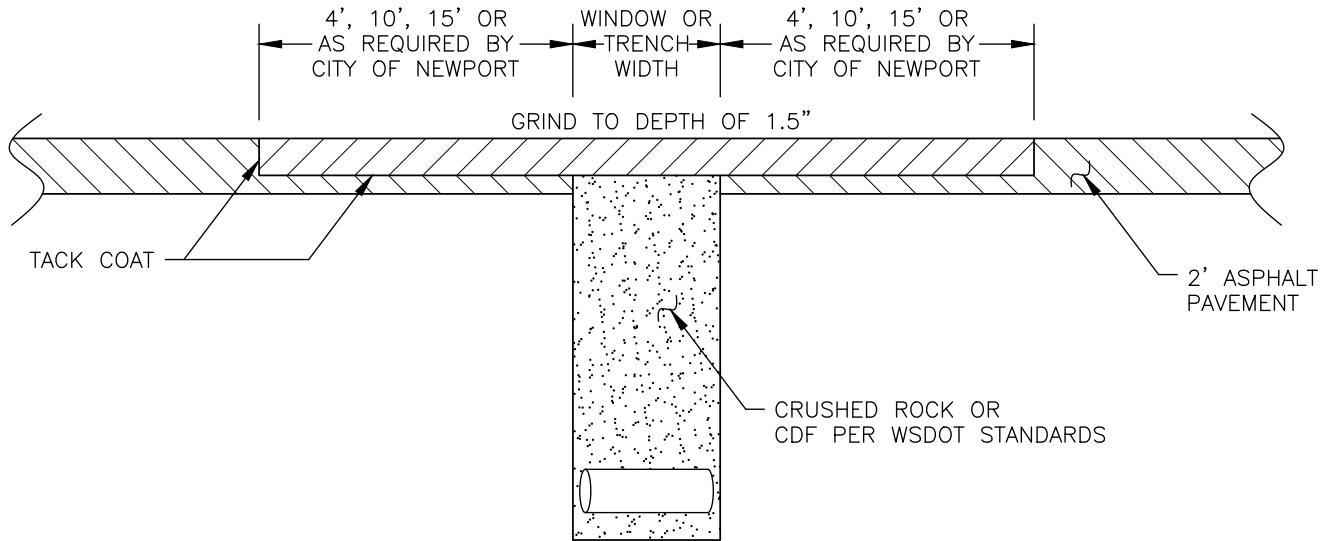
TRENCH REPAIR AND PAVEMENT RESTORATION
(OVERLAY NOT REQUIRED)

SCALE: NTS

DATE: 7-01-22

CITY OF NEWPORT STANDARD
STREET DETAIL

SHEET NO.
TR-3



NOTES:

- 1) THE OVERLAY SHALL EXTEND ACROSS THE ENTIRE LANE WIDTH.
- 2) THE REPAIR SHALL BE COMPLETED WITHIN THE TIME FRAME STATED ON THE PERMIT.
- 3) THE TEMPORARY PATCH OF COLD PATCH, GRAVEL, OR STEEL PLATES IF APPROVED SHALL BE INSTALLED AS STATED ON PERMIT. THE APPLICANT IS RESPONSIBLE FOR DAILY MAINTENANCE OF THE PATCH DURING THE INTERIM PERIOD.
- 4) SEAL JOINTS WITH RUBBERIZED ASPHALT SEALER.
- 5) SAND, GRAVEL SCREENING, OR PEA GRAVEL MAY BE USED AS BACKFILL WITHIN THE PIPE ZONE. THE REMAINDER OF THE EXCAVATION SHALL BE BACKFILLED WITH CRUSHED ROCK OR CDF PER WSDOT STANDARDS.

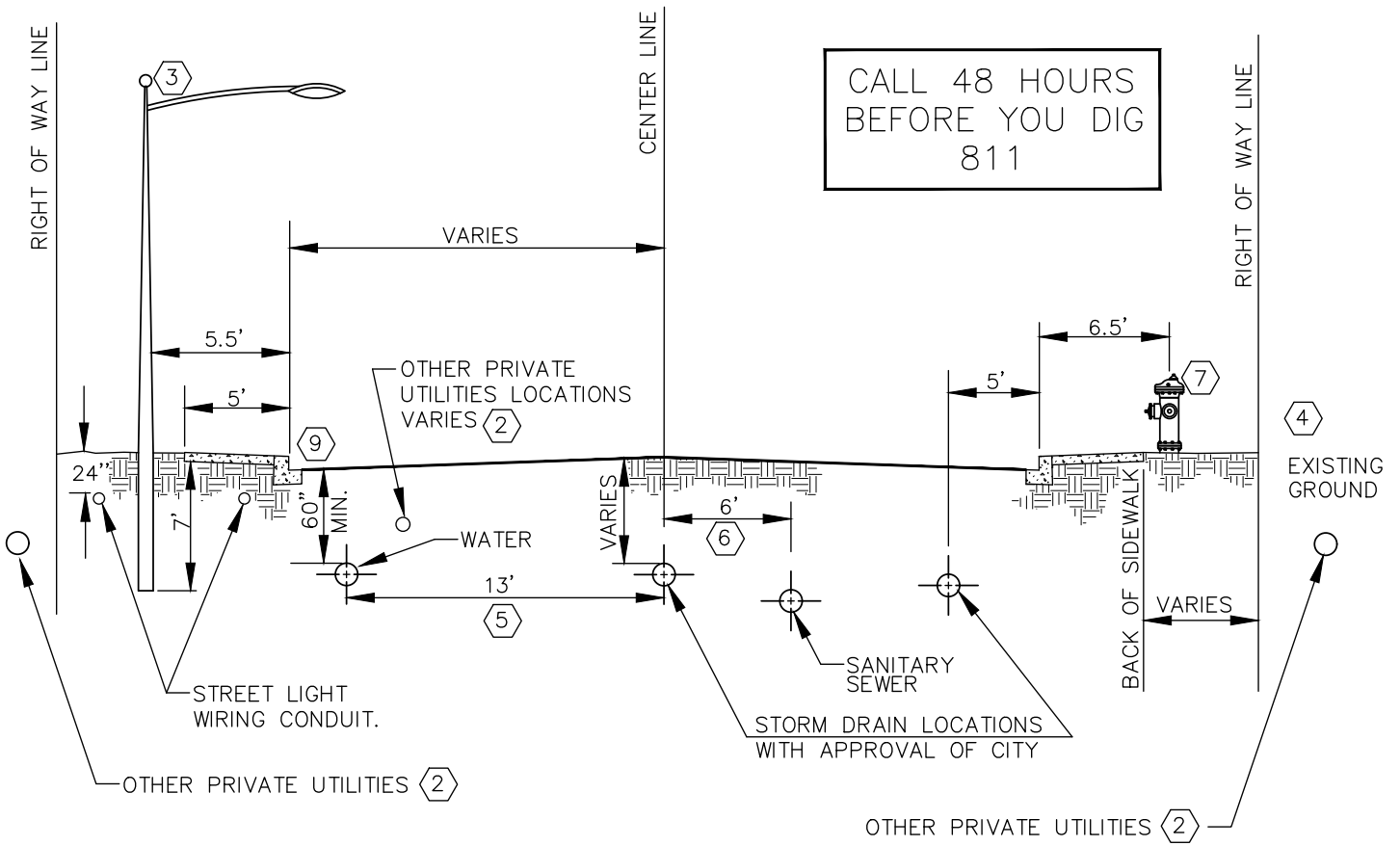
**WINDOW AND TRANSVERSE TRENCH REPAIR
(OVERLAY REQUIRED)**

SCALE: NTS

DATE: 7-01-22

**CITY OF NEWPORT STANDARD
STREET DETAIL**

SHEET NO.
TR-4

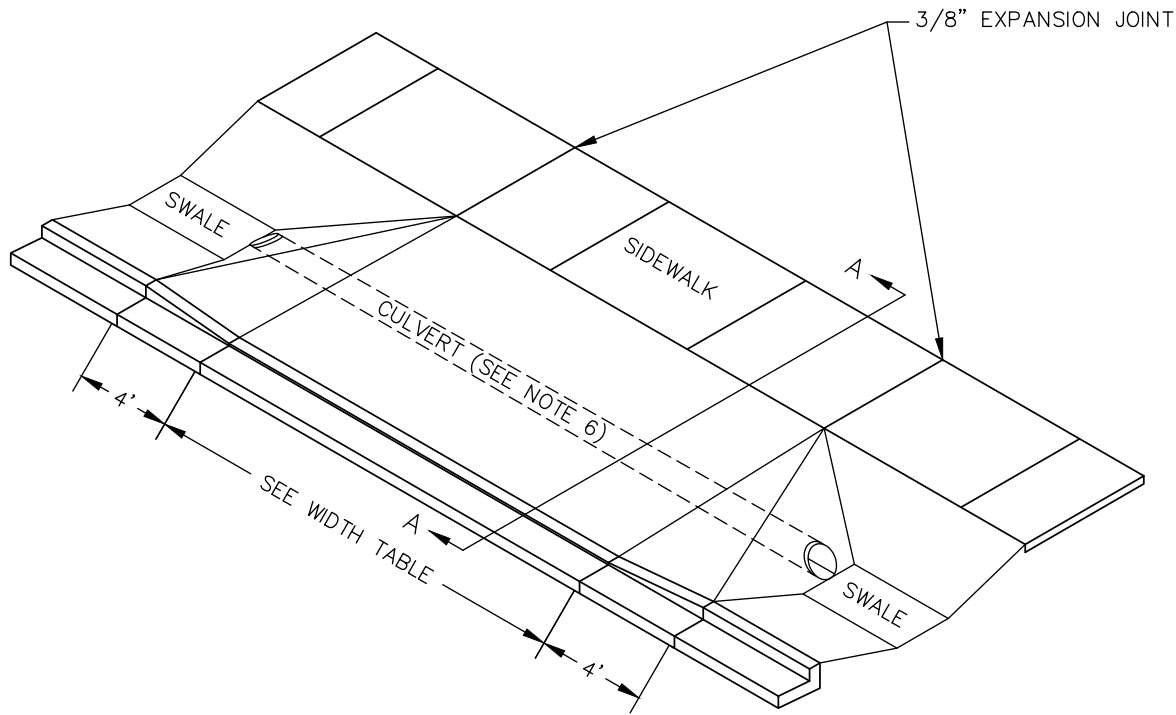


NOTES:

- ① DEVELOPER OR CONTRACTOR IS REQUIRED TO CALL 811 A MINIMUM OF 48 HOURS PRIOR TO DIGGING WITHIN THE LIMITS OF CITY RIGHT-OF-WAY FOR THE LOCATION MARKING OF ALL UNDERGROUND UTILITIES.
- ② TYPICAL LOCATIONS FOR OTHER PROPOSED OR EXISTING UTILITIES SHALL BE VERIFIED BY THE DEVELOPER OR CONTRACTOR. A MINIMUM HORIZONTAL SEPARATION OF 3 FOOT SHALL BE MAINTAINED FROM CITY WATER MAINS AND A MINIMUM 5 FOOT HORIZONTAL SEPARATION SHALL BE MAINTAINED FROM CITY SEWER AND STORM SEWER MAINS. UTILITIES LOCATED OUTSIDE RIGHT OF WAY REQUIRE AN EASEMENT.
- ③ STREET LIGHT POLES TYPICALLY WILL BE INSTALLED ON ALTERNATING SIDES OF THE STREET.
- ④ WATER SERVICE STOPS WILL BE INSTALLED AT THE PROPERTY LINE.
- ⑤ POTABLE WATER LINES TYPICALLY SHALL BE INSTALLED 13 FEET FROM AND PARALLEL TO THE CENTERLINE OF THE RIGHT-OF-WAY ON THE NORTH OR WEST SIDE.
- ⑥ SANITARY SEWER LINE TYPICALLY SHALL BE INSTALLED 6 FEET FROM AND PARALLEL TO THE CENTERLINE OF THE RIGHT-OF-WAY ON THE SOUTH OR EAST SIDE.
- ⑦ FIRE HYDRANTS TYPICALLY WILL BE INSTALLED ON ALTERNATING SIDES OF THE STREET ON 300 FOOT SPACING.
- ⑧ GAS, POWER, TELEPHONE AND OTHER UTILITIES SHALL MAINTAIN A MINIMUM 3 FOOT HORIZONTAL CLEARANCE FROM CITY WATER LINES, AND 5 FOOT HORIZONTAL CLEARANCE FROM CITY SEWER LINES.
- ⑨ IF CONFLICTS REQUIRE ALTERNATE WATER OR SEWER MAIN LOCATIONS, APPROVAL SHALL BE OBTAINED FROM THE CITY FOR THE LOCATION. A MINIMUM 3 FOOT SEPARATION FROM THE FACE OF CURB IS REQUIRED.

| | | |
|---|--------------------------|---------------|
| TYPICAL CITY UTILITY LOCATION | SCALE: NTS | DATE: 7-01-22 |
| | | |
| CITY OF NEWPORT STANDARD STREET DETAIL | SHEET NO. TR-5 | |

DRIVEWAY AND APPROACH DETAILS

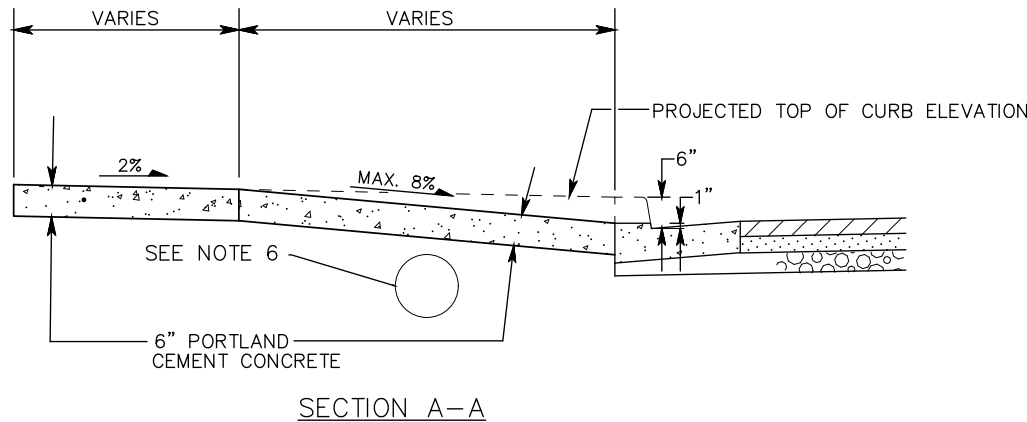


GENERAL NOTES

1. EXPANSION JOINT MATERIAL SHALL BE PLACED IN THE CENTER OF ALL DRIVEWAYS OVER 20' IN WIDTH.
2. EXPANSION JOINT REQUIRED IF POUR INCLUDES ADDITIONAL DRIVEWAY AREA.
3. CONCRETE FOR DRIVEWAYS SHALL BE CLASS 3000 AIR ENTRAINED.
4. ALL EXTERNAL EDGES TO BE TROWELLED WITH 1/4" RADIUS EDGER.
5. SIDEWALK TO BE SCORED EVERY 5'. 3/8" EXPANSION JOINT TO BE INSTALLED AT LOCATIONS WHERE SIDEWALK INTERSECTS OTHER SIDEWALKS AND DRIVEWAYS.
6. FOR ROADSIDE SWALE APPLICATIONS, INSTALL 12" CULVERT UNDER APPROACH PER SPOKANE REGIONAL STORMWATER MANUAL. ENDS SHALL BE BEVELED TO MATCH SWALE SLOPE.
7. MIN. 4" THICK CRUSHED SURFACING TOP COURSE LAYER UNDER DRIVE APPROACH.
8. FIRST 2' OF DRIVE APPROACH (AT CURB SIDE) SHALL BE THICKENED TO MATCH BOTTOM OF CURB.

DIMENSION TABLE

| TYPE | DISTANCES "W" | |
|-------|------------------|------|
| | MIN. | MAX. |
| COMM. | 30' | 40' |
| RES. | 16' | 30' |



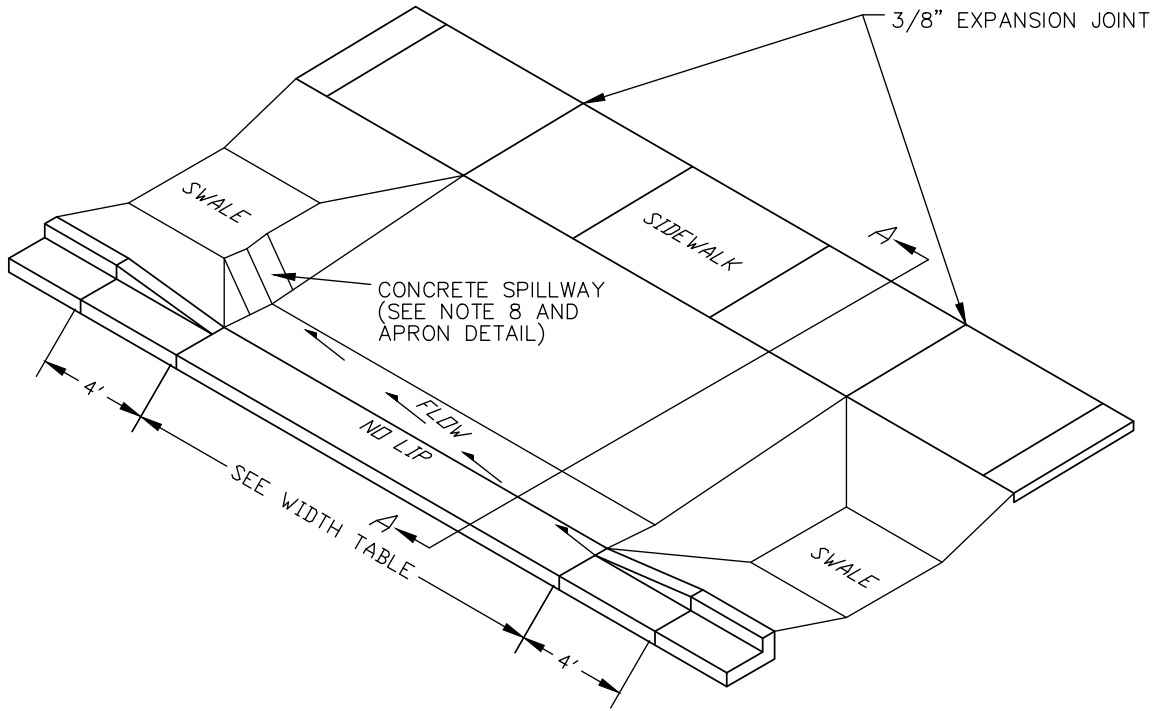
**DRIVEWAY APPROACH
SEPARATED SIDEWALK**

SCALE: NTS

DATE: 7-01-22

**CITY OF NEWPORT
STANDARD STREET DETAIL**

SHEET NO.
TR-6

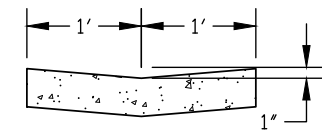
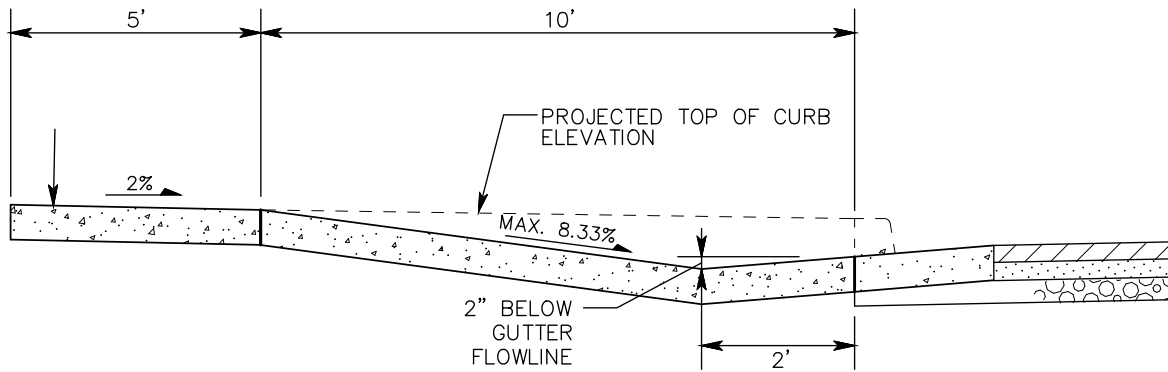


GENERAL NOTES

1. EXPANSION JOINT MATERIAL SHALL BE PLACED IN THE CENTER OF ALL DRIVEWAYS OVER 20' IN WIDTH.
2. EXPANSION JOINT REQUIRED IF POUR INCLUDES ADDITIONAL DRIVEWAY AREA.
3. CONCRETE FOR DRIVEWAYS SHALL BE CLASS 3000 AIR ENTRAINED.
4. ALL EXTERNAL EDGES TO BE TROWELLED WITH 1/4" RADIUS EDGER.
5. SIDEWALK TO BE SCORED EVERY 5'. 3/8" EXPANSION JOINT TO BE INSTALLED AT LOCATIONS WHERE SIDEWALK INTERSECTS OTHER SIDEWALKS AND DRIVEWAYS.
6. THIS APPLICATION IS FOR RESIDENTIAL USE ONLY.
7. MIN. 4" THICK CRUSHED SURFACING TOP COURSE LAYER UNDER DRIVE APPROACH.
8. APRON SHALL BE SECURED TO DRIVE APPROACH WITH TWO 12" #4 REBAR, EMBEDDED 6".

DIMENSION TABLE

| TYPE | DISTANCES "W" | |
|-------|------------------|------|
| | MIN. | MAX. |
| COMM. | 30' | 40' |
| RES. | 16' | 30' |



APRON DETAIL

SECTION A-A

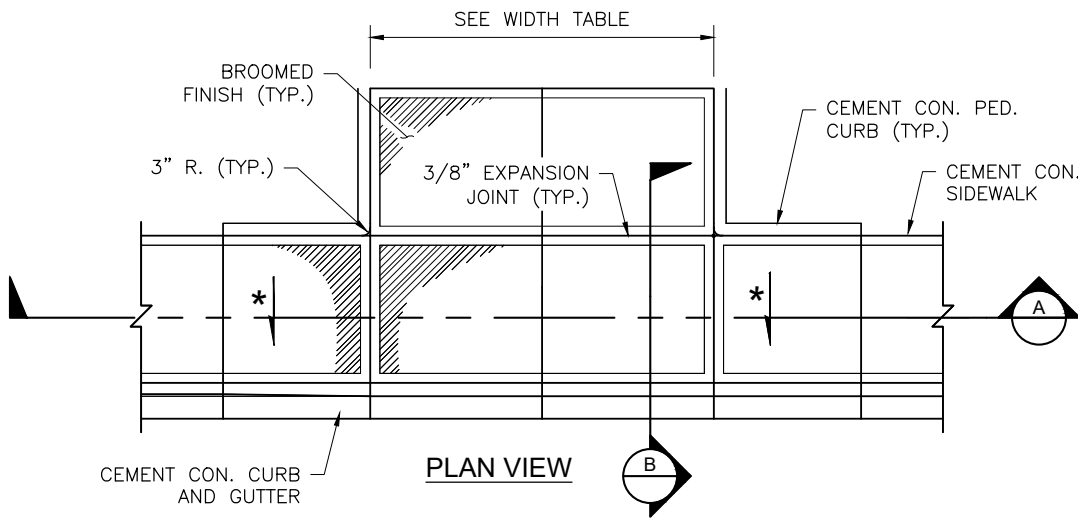
**DRIVEWAY APPROACH
SWALE INLET**

SCALE: NTS

DATE: 7-01-22

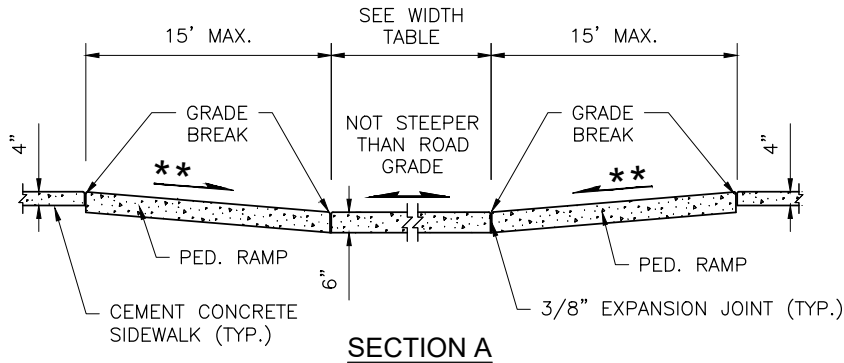
**CITY OF NEWPORT
STANDARD STREET DETAIL**

SHEET NO.
TR-7

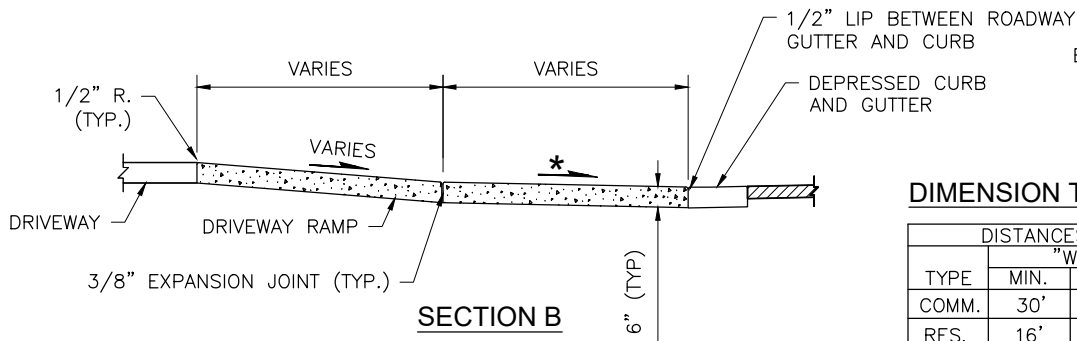


GENERAL NOTES

1. EXPANSION JOINT MATERIAL SHALL BE PLACED IN THE CENTER OF ALL DRIVEWAYS OVER 20' IN WIDTH.
2. EXPANSION JOINT REQUIRED IF POUR INCLUDES ADDITIONAL DRIVEWAY AREA.
3. CONCRETE FOR DRIVEWAYS SHALL BE CLASS 3000 AIR ENTRAINED.
4. ALL EXTERNAL EDGES TO BE TROWELED WITH 1/4" RADIUS EDGER.
5. SIDEWALK TO BE SCORED EVERY 5'. 3/8" EXPANSION JOINT TO BE INSTALLED AT LOCATIONS WHERE SIDEWALK INTERSECTS OTHER SIDEWALKS AND DRIVEWAYS.
6. MIN. 4" THICK CRUSHED SURFACING TOP COURSE LAYER UNDER DRIVE APPROACH.
7. FIRST 2' OF DRIVE APPROACH (AT CURB SIDE) SHALL BE THICKENED TO MATCH BOTTOM OF CURB.
8. AVOID PLACING DRAINAGE STRUCTURES, JUNCTION BOXES OR OTHER OBSTRUCTIONS IN FRONT OF DRIVEWAY ENTRANCES.
9. WHERE "GRADE BREAK" IS CALLED OUT, THE ENTIRE LENGTH OF THE LINE BETWEEN THE TWO ADJACENT SURFACE PLANES SHALL BE FLUSH.
10. THE PEDESTRIAN RAMP LENGTH IS NOT REQUIRED TO EXCEED 15 FEET. WHEN APPLYING THE 15-FOOT MAX. LENGTH (MEASURED FROM BACK OF SIDEWALK) THE RUNNING SLOPE OF THE PEDESTRIAN RAMP IS ALLOWED TO EXCEED 8.3%. USE A SINGLE CONSTANT SLOPE FROM BOTTOM OF RAMP TO TOP OF RAMP TO MATCH INTO THE SIDEWALK OVER A HORIZONTAL DISTANCE OF 15 FEET.

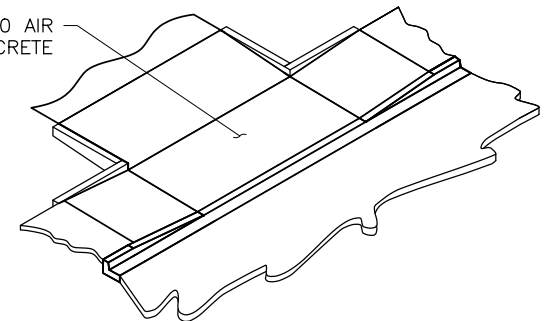


- ← SLOPE IN EITHER DIRECTION
- * 1.5% OR FLATTER RECOMMENDED FOR DESIGN/FORMWORK (2% MAX.)
- ** 7.5% OR FLATTER RECOMMENDED FOR DESIGN/FORMWORK (8.3% MAX.)



DIMENSION TABLE

| TYPE | DISTANCES "W" | |
|-------|------------------|------|
| | MIN. | MAX. |
| COMM. | 30' | 40' |
| RES. | 16' | 30' |



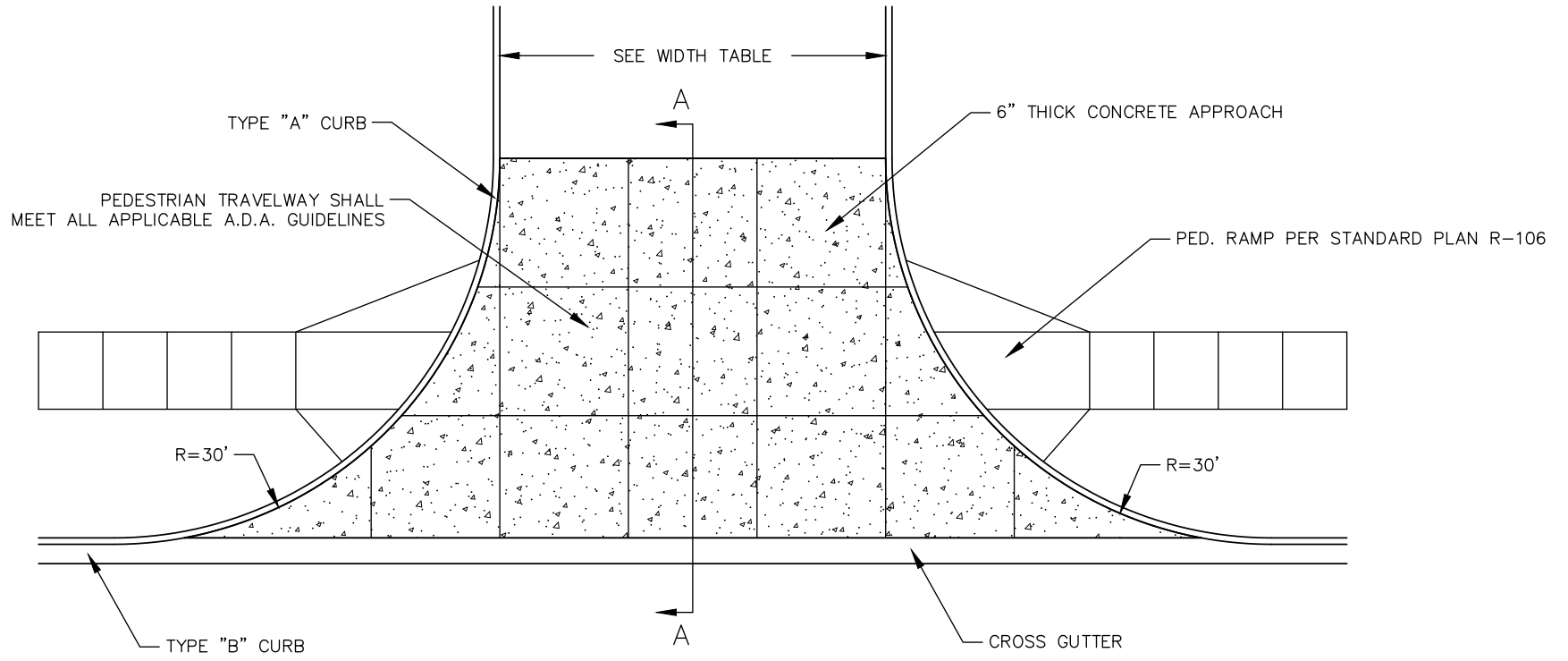
**DRIVEWAY APPROACH
ADJACENT SIDEWALK**

SCALE: NTS

DATE: 07-01-2022

**CITY OF NEWPORT
STANDARD STREET DETAIL**

SHEET NO.
TR-8



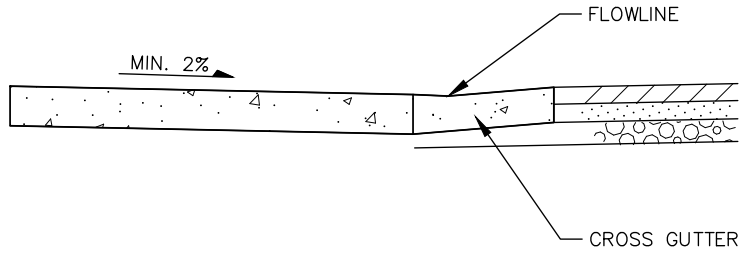
PLAN

DIMENSION TABLE

| TYPE | DISTANCES "W" | |
|-------|------------------|------|
| | MIN. | MAX. |
| COMM. | 30' | 40' |

GENERAL NOTES

1. ONLY TO BE USED WITH APPROVAL BY COMMUNITY SERVICES DIRECTOR.
2. CONCRETE FOR DRIVEWAYS SHALL BE CLASS 3000 AIR ENTRAINED.
3. ALL EXTERNAL EDGES TO BE TOWELED WITH 1/4" RADIUS EDGER.
4. MIN. 4" THICK COMPACTED CRUSHED SURFACING TOP COURSE LAYER UNDER DRIVE APPROACH.



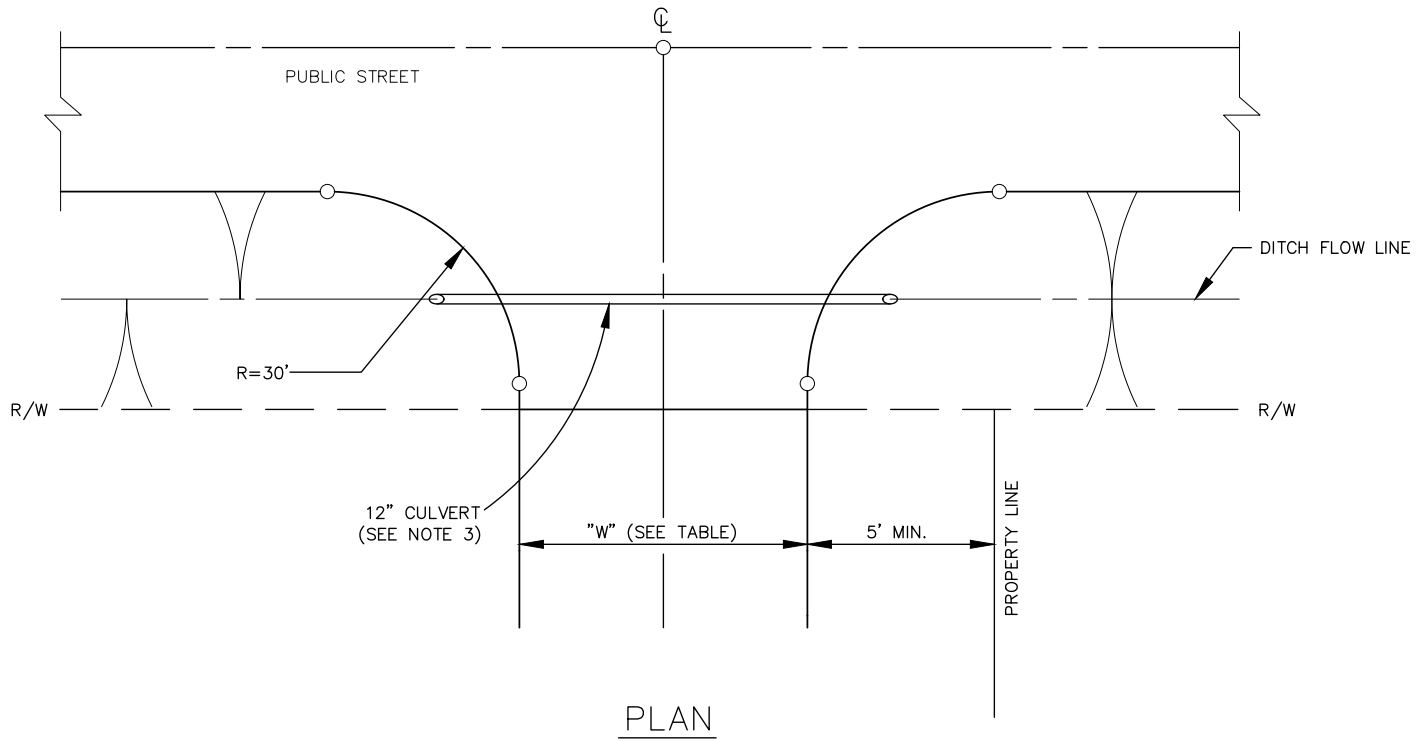
SECTION A-A

**DRIVEWAY APPROACH
HIGH VOLUME**

SCALE: NTS DATE: 7-01-22

**CITY OF NEWPORT
STANDARD STREET DETAIL**

SHEET NO.
TR-9



GENERAL NOTES

1. ONLY TO BE USED WITH APPROVAL BY DEVELOPMENT SERVICES SENIOR ENGINEER.
2. SHALL BE CONSTRUCTED OF MIN. 3" ACP OVER 6" CRUSHED SURFACING TOP COURSE.
3. INSTALL 12" CULVERT UNDER APPROACH PER SPOKANE REGIONAL STORMWATER MANUAL. ENDS SHALL BE BEVELED TO MATCH DITCH SLOPE.

DIMENSION TABLE

| TYPE | DISTANCES | |
|-------|-----------|------|
| | "W" | |
| | MIN. | MAX. |
| COMM. | 30 | 40' |
| RES. | 16' | 30' |

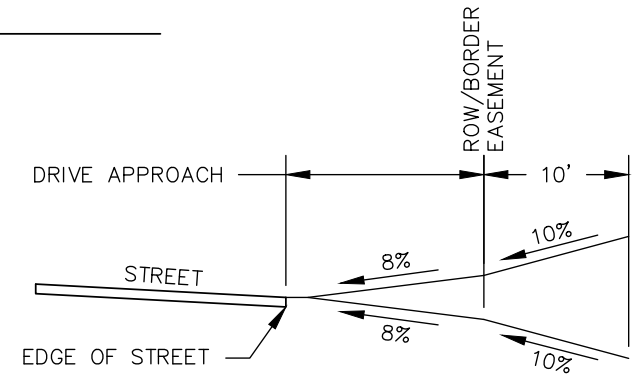
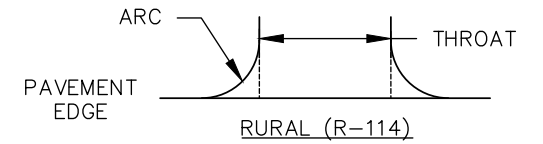
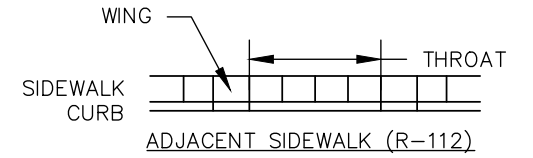
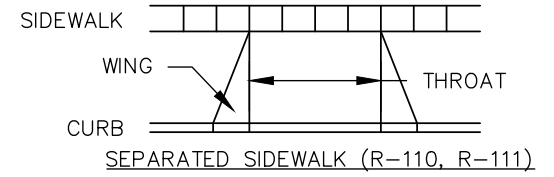
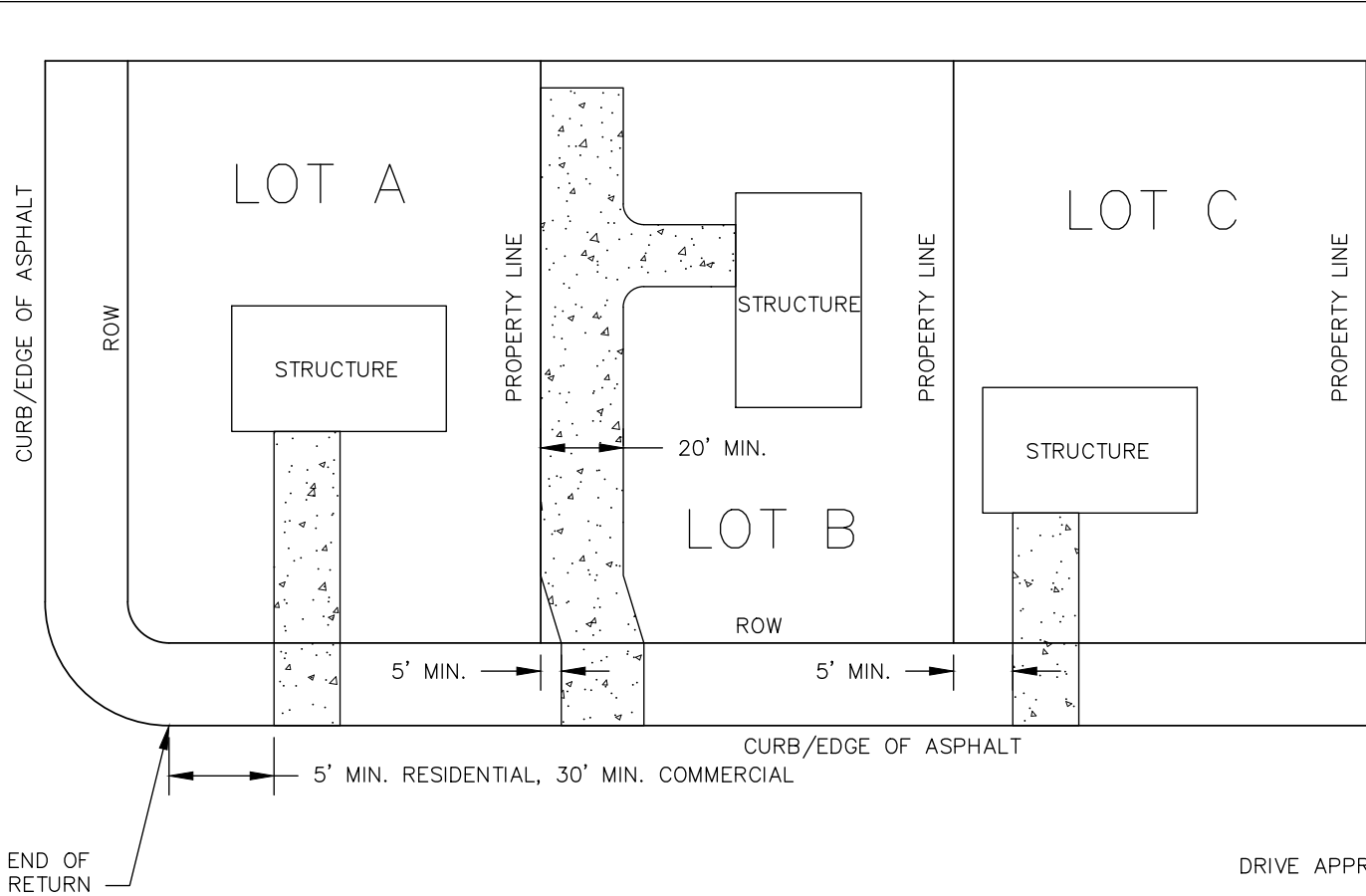
**DRIVEWAY APPROACH
RURAL**

SCALE: NTS

DATE: 7-01-22

**CITY OF NEWPORT
STANDARD STREET DETAIL**

SHEET NO.
TR-10



GENERAL NOTES

1. LOT "A" DIMENSIONS ARE FOR CORNER LOTS.
2. LOT "B" DIMENSIONS ARE FOR DRIVEWAYS EQUAL TO OR IN EXCESS OF 150' MEASURED FROM CURB/EDGE OF ASPHALT TO FRONT DOOR OF STRUCTURE. SEE STANDARD PLAN R-133 FOR MINIMUM TURNAROUND DIMENSIONS.
3. LOT "C" DIMENSIONS ARE FOR ALL OTHER APPLICATIONS.
4. PROPERTY LINE OFFSETS ARE TO EDGE OF THROAT (SEE THROAT DEFINITIONS ABOVE).
5. SEE STANDARD PLANS R-110 TO R-114 FOR ALLOWABLE DRIVE APPROACH TYPES.

**DRIVEWAYS
GENERAL REQUIREMENTS**

**CITY OF NEWPORT
STANDARD STREET DETAIL**

SCALE: NTS

DATE: 7-01-22

SHEET NO.
TR-11